

# HERITAGE IMPACT STATEMENT



## 74 Carlton Crescent Summer Hill

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## **1.0 INTRODUCTION**

### **1.1 Preamble**

This Heritage Impact Statement (HIS) has been prepared in conjunction with a Development Application for alterations and additions to the former Ambulance Station, No. 74 Carlton Crescent, Summer Hill, New South Wales, to create student accommodation.

The site is located within the Inner West Council area. The principal planning control for the site is the *Ashfield Local Environmental Plan 2013 (LEP 2013)*. The site is located within the Summer Hill Central Heritage Conservation Area as defined by Schedule 5 Part 2 of the *LEP 2013*. The site also lies within the vicinity of a number of heritage items identified by Schedule 5 Part 1 of the *LEP 2013*. Under Part 5.10 of the *LEP 2013*:

#### **(4) Effect of proposed development on heritage significance**

The consent authority must, before granting consent under this clause in respect of a heritage item or heritage conservation area, consider the effect of the proposed development on the heritage significance of the item or area concerned. This subclause applies regardless of whether a heritage management document is prepared under subclause (5) or a heritage conservation management plan is submitted under subclause (6).

#### **(5) Heritage assessment**

The consent authority may, before granting consent to any development:

- (a) on land on which a heritage item is located, or
  - (b) on land that is within a heritage conservation area, or
  - (c) on land that is within the vicinity of land referred to in paragraph (a) or (b),
- require a heritage management document to be prepared that assesses the extent to which the carrying out of the proposed development would affect the heritage significance of the heritage item or heritage conservation area concerned.

The appropriate heritage management document in this instance is a Heritage Impact Statement (HIS).

This statement has been prepared at the request of the owners of the site (Iglu No. 210 Pty Ltd ("Iglu")) and accompanies plans prepared by Bates Smart.

### **1.2 Authorship**

This statement has been prepared by Alice Fuller, B.Appl.Sc.(CCM), M.Herit.Cons.(Hons), and James Phillips, B.Sc.(Arch), B.Arch, M.Herit.Cons.(Hons), of Weir Phillips Heritage.

### **1.3 Limitations**

An Aboriginal history and assessment was not provided for. No historical archaeology was carried out on the site. A search of the ambulance service records held by NSW State Records was not provided for.

### **1.4 Methodology**

This HIS has been prepared with reference to the NSW Heritage Division publication *Statements of Heritage Impact* (2002 update) and with reference to the Inner West Council's planning documents listed under Section 1.5 below.

A site visit was carried out in October 2018. Unless otherwise stated, the photographs contained in this statement were taken at this time.

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## 1.5 Documentary Evidence

### 1.5.1 General References

- 'Ambulance Ball,' *The Daily Telegraph*, 29 May, 1924.
- 'Ambulance Station for the Western Suburbs,' *The Sydney Morning Herald*, 29 May, 1924.
- 'Ambulance Station, New Station Opened,' *The Sydney Morning Herald*, 15 December, 1924.
- *Ashfield 1871-1971*, NSW, Horwitz Publications, 1972.
- 'Ashfield Ambulance Waggon,' *The Sydney Morning Herald*, 23 May, 1910.
- Coupe, S. and R., *Speed the Plough: Ashfield 1788-1988*, The Council of the Municipality of Ashfield, 1988.
- 'For Sick and Injured,' *Evening News*, 24 August, 1923.
- Gerdtz, J., *Buick in the Ambulance Service of NSW*. Online reference.
- 'The Great Land Sale', *The Sydney Morning Herald*, 21 September, 1878.
- Hainsworth, D.R., 'Kable, Henry (1763-1846)', *Australian Dictionary of Biography*, National Centre of Biography, Australian National University, <http://adb.anu.edu.au/biography/kable-henry-2285/text2941>, published first in hardcopy 1967, accessed online 6 November 2018.
- JBS & G, *Health Infrastructure Phase 1 and Phase 2 Environmental Site Assessment, Summer Hill Ambulance Station*, October, 2017. Unpublished report provided by the client.
- 'Knitting', *The Sydney Morning Herald*, 21 July, 1937.
- 'New Ambulance Station,' *The Sydney Morning Herald*, 24 May, 1924.
- 'New Ambulance Station,' *Sun*, 14 December, 1924.
- Pratten, C., *Summer Hill*, Ashfield & District Historical Society Inc., 1999.
- '64 Miles Daily. W. Suburbs Ambulance. Annual Report,' *The Sun*, 11 September, 1926.
- 'Tenders Called', *Construction and Real Estate Journal*, 22 July, 1936.
- 'Western Suburbs Ambulance,' *The Sydney Morning Herald*, 24 October 1922.
- 'Western Suburbs Ambulance,' *Daily Telegraph*, 30 September, 1924.
- Western Suburbs Ambulance,' *Argus*, 17 October, 1924.
- 'Western Suburbs Ambulance,' *The Sydney Morning Herald*, 25 September, 1930.
- 'W. Suburbs Ambulance. More Money Needed,' *The Sun*, 10 September, 1926.
- 'Western Suburbs District Ambulance,' *Argus*, 5 December, 1925.

### 1.5.2 Historic Plans and Photographs

- Metropolitan Water, Sewerage & Drainage Board Plan, *Ashfield Sheet No. 27*, 11 November 1890. Ashfield Library, Local Studies Collection.
- NSW Lands Department, (*Aerial Photograph over Summer Hill*), 1943. SIX Maps.
- *Postcard from Summer Hill*, undated. Ashfield Library, Local Studies Collection.
- *The Underwood Estate, Ashfield...*, 23 September, 1878. National Library of Australia.

### 1.5.3 Heritage Listing Sheets

- *Daryl Jackson Memorial Gardens (public reserve)*, No. 12-131 Smith Street, Marrickville. State Heritage Inventory Database No.: 1020258.

#### 1.5.4 NSW LPI Information

- Certificates of Title Volume 354 Folio 134; Volume 554 Folio 51; Volume 666 Folio 207; Volume 788 Folios 177 to 180 inclusive; Volume 3372 Folio 213; Volume 3446 Folio 26; Volume 3663 Folio 72; and Volume 4751 Folio 25.

#### 1.5.5 Planning Documents

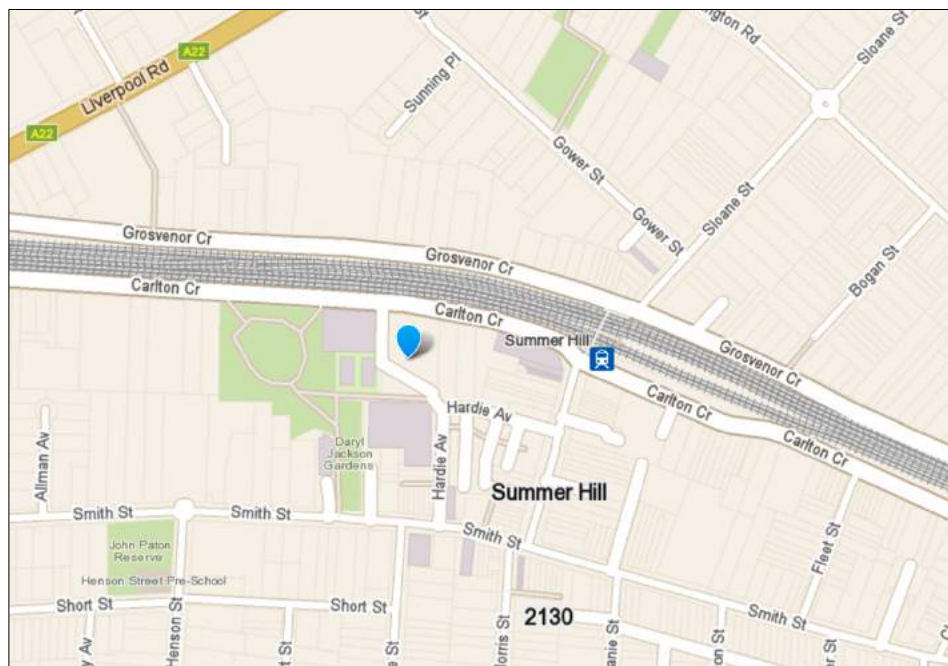
- *Ashfield Local Environmental Plan 2013.*
- *Inner West Comprehensive Development Control Plan for Ashbury, Ashfield, Croydon, Croydon Park, Haberfield, Hurlstone Park and Summer Hill 2016.*

#### 1.5.6 Other

- Ashfield Municipal Council, Building Files, Nos. 55/1676, 58/2522, 62/4175 and 72/8607. Inner West Council Archives.
- Ashfield Municipal Council Building Register for 1924. Ashfield Local Studies Collection.

#### 1.6 Site Location

No. 74 Carlton Crescent, Summer Hill is located on the south eastern corner of Carlton Crescent and Hardie Avenue (Figure 1). The site is identified as Lot 2.D.P.717782.



**Figure 1: Site Location.**

Whereis.com.au.

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## 2.0 BRIEF OUTLINE OF THE HISTORICAL DEVELOPMENT OF THE SITE

Until recently, the subject site lay within the boundaries of the Municipality of Ashfield, hence the references to the same below.

The following history focuses on the western part of the subject site and the interwar period ambulance station that stands on this part of the site.

### 2.1 Wangal and Cadigal Country

While an Aboriginal history is not provided for, it is acknowledged that present-day Inner West Council area is the traditional country of two bands of Dharug speaking people, the Wangal (Wongol or Wanegal) and the Cadigal (Kadigal).

### 2.2 The Site to 1880

The Colony of New South Wales was formerly established on 26 January, 1788 at Sydney Cove on the southern side of Sydney Harbour. A second township, Rosehill (later Parramatta), was established in November 1788. The following year, work began on a rough track, later Parramatta Road, to link the two settlements. European use of the area radiated outwards from Parramatta Road and, at a later date, from a second major road, the Great South Road (later Liverpool Road), work upon which started in 1813.

From January 1793, successive governors granted land outside the declared boundaries of the Township of Sydney in order to open up the land and augment the Colony's food supplies. Present day Ashfield lies well outside these boundaries. The first grant within the area later the Ashfield Municipality was a 100-acre grant, known as *Canterbury Farm* or *Canterbury Vale*, made to the Reverend Richard Johnson in 1793. Other early recipients of 100-acre grants in the area were Captain John Townsend (1794), Surveyor Augustus Alt (1794), Lieutenants John Piper and James Hunt Lucas (1794) and Captains Joseph Foveaux (1794) and William Paterson (1794). Smaller land grants, of between 14 and 30 acres, were made to emancipists and privates within the New South Wales Corps.

The subject site is located on a 30 acre grant made to the emancipist Henry Kable on 8 January, 1794.<sup>1</sup> Kable had arrived in NSW with the First Fleet serving a fourteen year sentence for theft. Within three years of this arrival, he had been made a constable and night watchmen. Within six years, he had become chief constable, a position he subsequently lost through misbehaviour. Kable developed private business interests and extensive landholdings. Kable went on to consolidate an estate of over 175 acres through grant and purchase within the present day Inner West Council area.<sup>2</sup>

By the late 1820s, four men -Robert Campbell, Simeon Lord, Henry Kable and Joseph Underwood - had consolidated substantial estates in the former Ashfield Municipal area. These four estates would determine land-use patterns into the second half of the nineteenth century.

### 2.3 James Underwood and the Underwood Estate

Sometime after the 1820s, the subject site became part of a second large estate, one consolidated by Joseph Underwood's brother, James Underwood. The estate became known as the *Underwood Estate* and would remain in the hands of James Underwood (and later his heirs) until the early 1880s. James Underwood had also arrived in New South Wales as a convict between 1788 and 1791. Underwood became a successful boat builder and businessman.

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<sup>1</sup> Certificate of Title Volume 3663 Folio 72. NSW LPI.

<sup>2</sup> For further information see D. R. Hainsworth, 'Kable, Henry (1763–1846)', *Australian Dictionary of Biography*, National Centre of Biography, Australian National University, <http://adb.anu.edu.au/biography/kable-henry-2285/text2941>, published first in hardcopy 1967, accessed online 6 November 2018.

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The foundations of the former Ashfield Municipality area, in which the subject site lay, were laid during the period in which James Underwood owned the Underwood Estate. The first major development occurred when a small section of Joseph Underwood's *Ashfield Park*, close by the junction of Parramatta and Liverpool Roads, was subdivided and offered for sale as the Village of Ashfield in 1838. Robert Campbell also began subdivision of his estate, in an area between Liverpool Road and Norton Street, around the same time (South Ashfield). The present-day Ashfield Town Centre evolved from these two villages.

When James Underwood died in 1844, he left a complex will that tied up his estate in prolonged litigation. The matter was settled by the introduction of a private member's bill into the NSW Legislative Assembly in 1873, opening the way for the subdivision of the estate.

The *Underwood Estate* was released for sale at a fortuitous time. During the period between James Underwood's death and the *Underwood Estate Act*, Ashfield had made considerable progress. Ashfield Station was one of five stations on the Sydney to Parramatta Junction railway line when it opened in 1855. The improved access into the area led to the construction of a number of substantial villas, typically occupied by those whose interests required their frequent presence in Sydney but who sought a country lifestyle. These villas were, however, the exception rather than the rule. Ashfield of the 1860s was predominately the domain of market gardeners, horticulturalists and tradespeople; large areas remained heavily wooded. Development was never even across the area. The greater part remained primarily agricultural in its pursuits or, in the case of the vast Ramsay Estate (present-day Haberfield) virgin bush. As described by the *New South Wales Gazetteer* in 1866:

'Ashfield....is an agricultural district, the greater portion consisting of good arable land, well suited for market gardening, which branch of industry is extensively carried out in the neighbourhood....at no distant day (it) is destined to become a place of consideration importance.'<sup>3</sup>

As the population grew, services improved. The first Post Office opened in 1856, followed in 1862 by a school and the appointment of a constable. Churches were consecrated and businesses opened. By 1871, sufficient progress had been made for the area to be incorporated as a Borough, later the Municipality of Ashfield.

The new Borough continued to grow. During the 1880s, the population of the Borough of Ashfield more than doubled from just over 4,000 people to over 11,000 people. This rapid rate of development was repeated throughout Sydney's suburbs as the population grew rapidly; by 1891, the suburbs would house one and a half times the population of the City.<sup>4</sup>

Services continued to improve. In 1884, in a move that was revolutionary at that time, Council established a system of dry earth closets that were serviced nightly. In 1887, a successful application was made for a Post Office at Ashfield and in the same year a School of Arts was built on the corner of Liverpool Road and Holden Street. In 1888, the Water, Sewerage and Drainage Board commenced the extension of reticulated water to Ashfield. In 1890, a special bill was passed authorising the construction of the Iron Cove and Long Cove stormwater channels.<sup>5</sup> Development, however, continued to be uneven across the area and large sections of the Borough retained a rural atmosphere.

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<sup>3</sup> Cited in S. and R. Coupe, *Speed the Plough: Ashfield 1788-1988*, The Council of the Municipality of Ashfield, 1988, p.69.

<sup>4</sup> *Ibid*, p.76.

<sup>5</sup> *Ashfield 1871-1971*, NSW, Horwitz Publications, 1972, p.46.

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## 2.4 Subdivision of the Underwood Estate

When the first eight parts of the *Underwood Estate* were released for sale in the late 1870s, it was lavishly praised in advertisements in the contemporary press. One advertisement, for example, described the estate as being 'in the neighbourhood of the City'; as lying 'situated on a natural and beautiful elevation'; as possessing 'delightful scenery, diversified and extensive'; as being in close proximity to the railway; and as an 'unrivalled spot' for suburban residences.<sup>6</sup>

The estate was frequently described as being located within 'Summer Hill', thought to be a corruption of 'Sunning Hill', the name of Nicholas Bayly's grant on the opposite side of Parramatta Road (now the suburb of Haberfield). For unknown reasons, the name had gradually come to be associated with the area covered by the *Underwood Estate* during the mid-nineteenth century.<sup>7</sup> The name was adopted for present-day Summer Hill Station when it opened as Summer Hill Platform in September 1879.

The area of the *Underwood Estate* to the south of the Summer Hill Platform was offered for sale in 1878-1880 (refer to Figure 3). The land was owned at this time by a partnership comprising William George Pennington, William Henry McKenzie, John Piper Mackenzie, Robert John King and Charles Tye Weeks.<sup>8</sup> The site is located in Section 7 of this subdivision. Sloane, Dover, Gower and Lackey Streets, Grosvenor and Carlton Crescents and Kensington Road had been laid out and named, at least on paper.<sup>9</sup>

The subject site comprises several lots within Section 7 of the above subdivision. Lot 36 Section 7 is the lot of primary interest to this statement, being the lot on which the majority of the interwar period Ambulance Station building now stands. Lots 34, 35 and 36 Section 7 were purchased from the estate subdivision by John McMinn, a watchmaker and jeweller in 1880.<sup>10</sup> The land would remain in the hands of the McMinn family until the 1920s. The last owner from the McMinn family, Wilfred McMinn, was registered as the owner of this land in January 1923.<sup>11</sup>

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<sup>6</sup> 'The Great Land Sale', *The Sydney Morning Herald*, 21 September, 1878.

<sup>7</sup> Sheena and Robert Coupe, *op.cit.*, 1988, p.35.

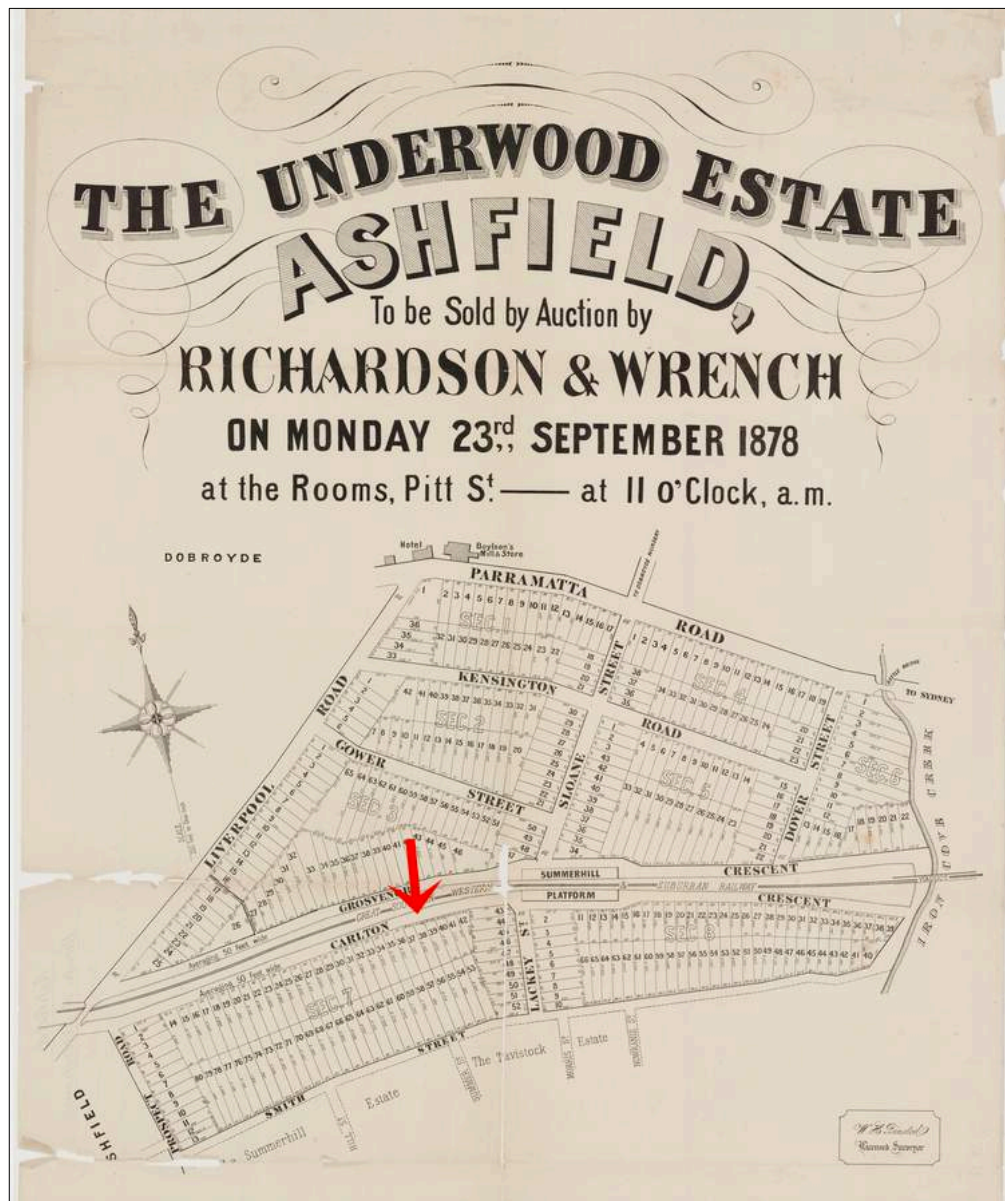
<sup>8</sup> Certificate of Title Volume 354 Folio 134. NSW LPI.

<sup>9</sup> C. Pratten, C., *Summer Hill*, Ashfield & District Historical Society Inc., 1999, p. 18

<sup>10</sup> Transfer attached to Certificate of Title Volume 354 Folio 134. NSW LPI.

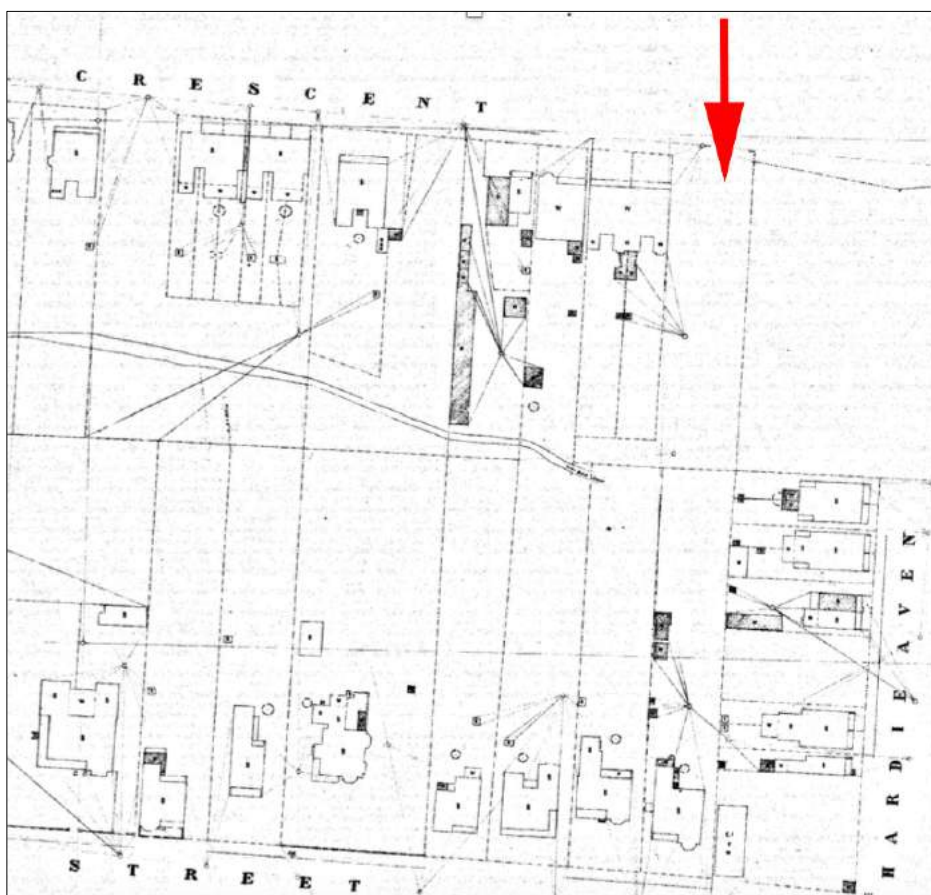
<sup>11</sup> Various Certificates of Title including Volume 554 Folio 51; Volume 788 Folios 177 to 180 inclusive; Volume 3372 Folio 213. NSW LPI.





**Figure 2: Richardson & Wrench, (*Underwood Estate, Ashfield*), 1878.**  
National Library of Australia.

It is not clear what the McMinn family used the land for. A plan of the area prepared in November 1890 for the Metropolitan Water, Sewerage and Drainage Board demonstrates that, while a number of attached and detached dwellings had been built fronting Carlton Crescent by this time, the subject site was vacant land. Council valuation records would need to be accessed to ascertain what, if anything, was constructed on the land prior to the existing interwar-period Ambulance Station building. Street numbers in *John Sands' Sydney and Suburban Directories* for this section of Carlton Crescent through the late nineteenth and first part of the twentieth century are difficult to match to the existing streetscape. There would, however, appear to be no listings in *Sands' Directories* for the subject site through to the early 1920s.



**Figure 3: Detail of the Metropolitan Water, Sewerage & Drainage Board Plan, Ashfield Sheet No. 27, 11 November 1890.**

Ashfield Local Studies Collection.

## 2.5 Western Part of the Site

### 2.5.1 Sale from the Estate

Lot 36 and a small part of Lot 35 Section 7 of the Underwood Estate were transferred to William Albert Travers (of Summer Hill, marine engineer), George James Rochester (of Burwood, fish merchant), William Laudenbach (of Ashfield, gem merchant), George Sydney Rolfe Porter (of Sandringham, clerk), Herbert Wallace Taylor (of Ashfield, fitter), John William Cordin (of Summer Hill, grocer) and Cutherbert William Robson (of Sydney, solicitor) as joint tenants on 13 April, 1923.<sup>12</sup> Other sources (see below) identify these men as being involved with the Western Suburbs Ambulance Association. It is on this land that the existing interwar period Ambulance Station would be erected.

### 2.5.2 Early Ambulance Services in Ashfield

Ambulance services have been provided in New South Wales by the government since at least 1894. The major provision of these services, however, fell to voluntary organisations such as the St. John Ambulance Association and Brigade and the Civic Ambulance Transport Brigade. In 1901, the residents of Summer Hill and Ashfield decided to establish an ambulance litter and branch of the St. Johns Ambulance Brigade in the district. The St. Johns Ambulance Brigade had been established in England in 1877 by the Order of St. John to address the growing need for effective first aid training to deal with the increased accidents brought about by an industrialised society. The Association spread to Australia in the 1880s.

<sup>12</sup> Transfer attached to Certificate of Title 3372 Folio 213. NSW LPI.

An office for the local St Johns Ambulance Brigade, and a shed to house their first motorised vehicle, were erected nine years later May 1910, in Carlton Crescent, near the Summer Hill Railway Station. The site was not considered to be ideal because of the passing trains and street traffic, but it was centrally located (Figure 4).<sup>13</sup>



**Figure 4: Postcard from Summer Hill, undated.**

The first ambulance station is visible in the top right hand corner.

Ashfield Library.

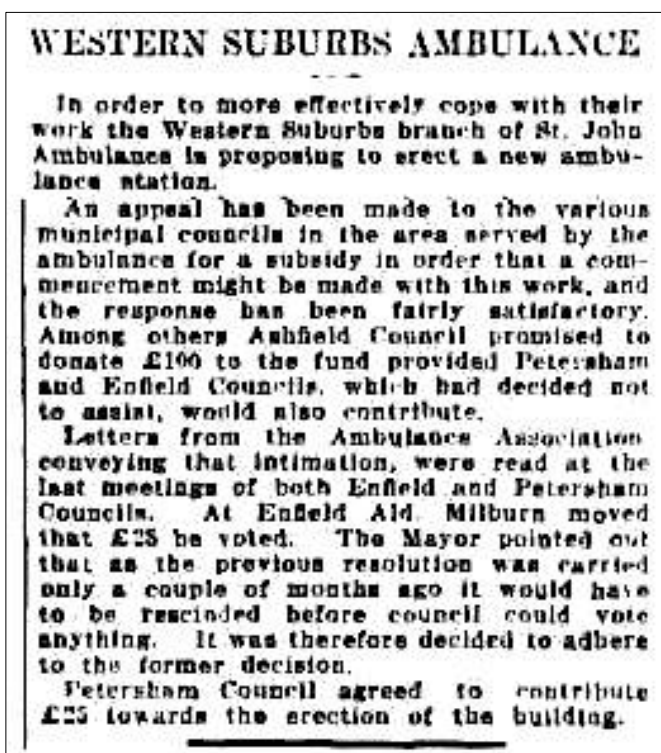
Changes to the way in which ambulance services were run in New South Wales began to occur following World War I when the *Ambulance Transport Service Act 1919* was passed. Under the Act, the Ambulance Transport Service Board was established to organise and control ambulance services. The Act provided for the establishment of Ambulance Districts administered by District Committees under the control of the Board. Ambulance districts were supported financially by member contributions. The District Committees were elected by contributors. Ashfield was located in the Western District. Newspaper advertisements and reports through the first half of the twentieth century show how the local community of the Western Districts were closely involved in raising funds for their ambulance services.<sup>14</sup>

### 2.5.3 Planning and Constructing the New Summer Hill Ambulance Station

The St. Johns Ambulance Association continued to work alongside the state following the passage of the 1919 Act. In 1922, as a result of a growing need to cope with the increase in work in the western suburbs, the Western Suburbs Branch of St. Johns Ambulance Service made an appeal to the local councils in the area to assist in erecting a new ambulance station. Several councils contribute funds to the building effort, including Ashfield Council (refer to Figure 5).

<sup>13</sup> 'Ashfield Ambulance Waggon,' *The Sydney Morning Herald*, 23 May, 1910, p.7.

<sup>14</sup> Refer, for example to 'Ambulance Ball,' *The Daily Telegraph*, 29 May, 1924, p.3.



**Figure 5:**  
**Appeal for Funds.**  
*The Sydney Morning Herald*, 24 October 1922.

The need for the new service was acute. In February 1922, the *Daily Telegraph* observed that:

'Ashfield, which is now celebrating its jubilee, is one of the most progressive suburbs in and around Sydney. Its growth and development in the last 50 years has been remarkable, due to judicious government and economical management of finances by the civic fathers, many of whom have been prominent business men of Sydney....

In 1875 there were 400 houses and the population of the borough had increased to 2000. Today, there are 23,657 residents and over 60 miles of streets....

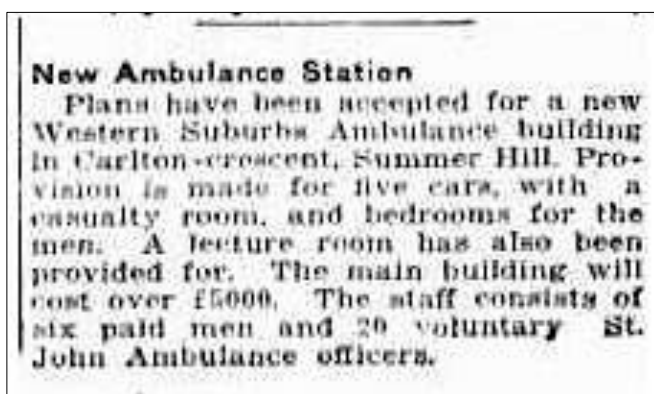
There has been an increase in recent years in valuation.....The improved capital for last year was £6,457,417, as against £4,584,076 in 1916....and £4,399,845 in 1914...'<sup>15</sup>

On 28 April, 1924 the NSW Ambulance Western Suburbs Division made an application to construct an ambulance station on the western part of the subject site. No architect or builder is identified. The structure is described in the Building Register as being of brick with 14 rooms and a tile roof. The estimated cost of construction was £4,875. The plans were approved in June, with work commencing soon after.<sup>16</sup> Figure 6 reproduces an article from *The Sydney Morning Herald* describing the new station.

<sup>15</sup> *Ashfield 1871-1971*, 1971, pp.50-51.

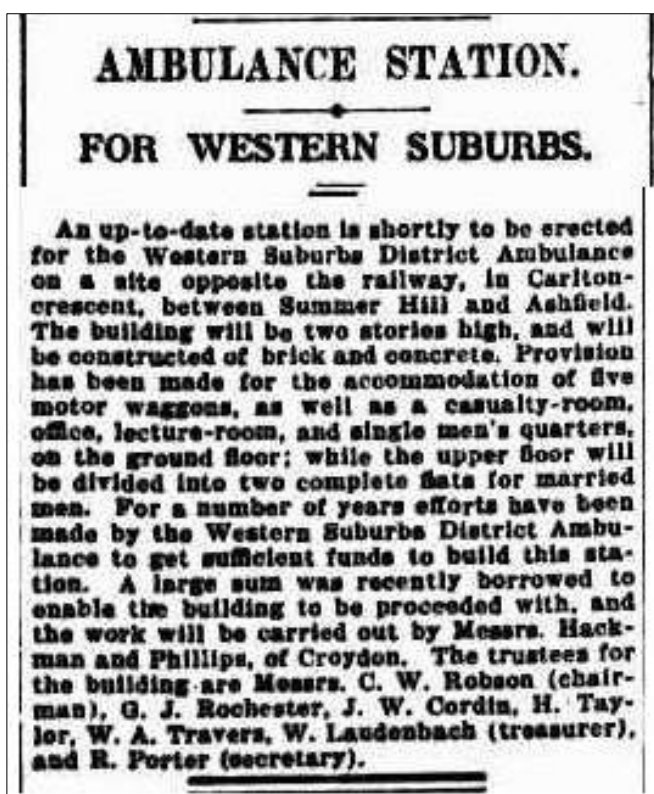
<sup>16</sup> Information from the Ashfield Municipal Council Building Register for 1924. Application No. 2424. Ashfield Local Studies Collection.





**Figure 6:**  
**The New Ambulance Station.**  
*The Sydney Morning Herald*, 24 May, 1924.

An article in *The Sydney Morning Herald* of 29 May 1924, provides further details (Figure 7). Note the name of the Trustees, being the names on the Certificate of Title. The article identifies the builder as Messrs. Hackman and Phillips of Croydon. Nothing is known of these builders; their name occasionally appears in newspapers for minor construction works in the period when the ambulance station was erected.



**Figure 7:**  
**The New Ambulance Station.**  
*The Sydney Morning Herald*, 29 May 1924.

In September 1924, Mr. W.H Bundy was appointed as the first superintendent of the new station.<sup>17</sup> By mid-October the building was nearing completion (refer to Figure 8).

<sup>17</sup> 'Western Suburbs Ambulance,' *Daily Telegraph*, 30 September, 1924, p.8.



Figure 8:  
Nearing completion.  
*Argus*, 17 October, 1924.

In early December 1924, the local newspaper, *The Argus*, announced that the official opening of the Ambulance Station would occur on 13 December, 1924 (Figure 9).

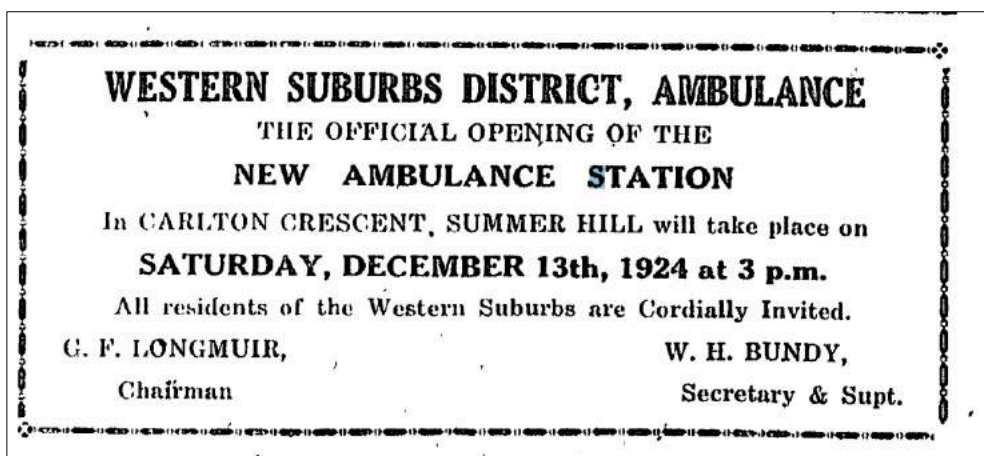


Figure 9: Notice in the local newspaper, 1924.  
*The Argus*, 5 December, 1925.

#### 2.5.4 Opening of the New Station

The opening of the new station was performed by the Minister for Health, Mr. Oakes on 13 December, 1924 was duly reported in *The Sydney Morning Herald* two days later.<sup>18</sup> The station opened with three well equipped motor ambulances, four drivers and one collector, under the charge of Superintendent W.H. Bundy. A large number of 'paper bricks' were sold at the opening to help pay for the building.<sup>19</sup> Figure 10 illustrates the type of ambulances that were in use by the Western District Ambulance Service, and hence housed within the building, at the time the Station was opened.

<sup>18</sup> 'Ambulance Station, New Station Opened' (1924, December 15). *The Sydney Morning Herald* (NSW : 1842 - 1954), , p. 12. National Library of Australia.

<sup>19</sup> 'New Ambulance Station,' *The Sun*, 14 December, 1924, p.5.



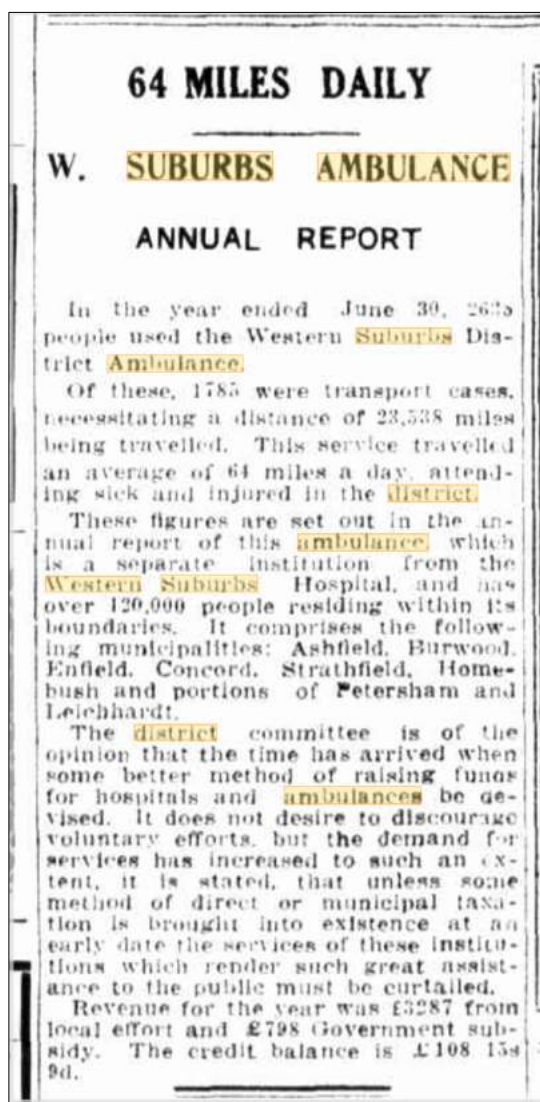
**Figure 10: Ambulances in use at the time the Station was opened.**

*Evening News*, 24 August, 1923, p.8.

The new ambulance station would appear to have operated at capacity from the first. During the year 1925-6, the Western Suburbs Ambulance Brigade conveyed 2,635 patients 23,538 miles, which was 500 cases and 400 miles above the figures for the year before.<sup>20</sup> The numbers would continue to increase. In 1930, it was reported that the Western District Ambulance had dealt with 4,427 cases and travelled 43,902 miles in the last year.<sup>21</sup> Ongoing funding continued to be an issue (Figure 11).

<sup>20</sup> 'W, Suburbs Ambulance. More Money Needed,' *The Sun*, 10 September, 1926, p.15.

<sup>21</sup> 'Western Suburbs Ambulance,' *The Sydney Morning Herald*, 25 September, 1930, p.15.



**Figure 11:**  
**Growth and the need for funds.**  
*The Sun*, 11 September, 1926..

Figure 12 provides an aerial photograph over the site dated 1930, which shows the form of the Ambulance Station soon after it was built. Note how the eastern part of the subject site is vacant land at this time. A small section of the adjoining lot to the east of the site of the recently erected Ambulance Station was acquired by the Ambulance Transport Service Board in December 1935.<sup>22</sup>

<sup>22</sup> Transfer attached to Certificate of Title Volume 666 Folio 207. NSW LPI. Note: the ownership of Lot 36 and part of Lot 35 (the land on which the original Station stood) had been transferred to the NSW Ambulance Transport Board in 1926.



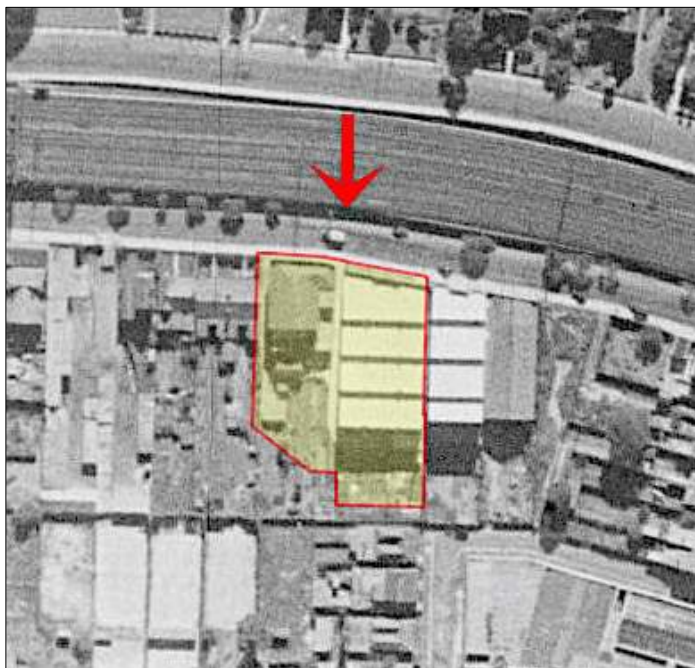


**Figure 12:**  
**Aerial photograph over**  
**subject site in 1930.**

Cited in JBS & G, *Health Infrastructure Phase 1 and Phase 2 Environmental Site Assessment, Summer Hill Ambulance Station*, October, 2017.

### 2.5.5 Later Alterations and Additions

In 1936, tenders were called for 'remodelling' the Western Suburbs District Ambulance Station by architects Rudder & Grout. Ashfield Council do not hold any plans; what this work was is unknown.<sup>23</sup> Figure 13 provides an aerial photograph over the site dated 1943. It would appear from a comparison of Figures 12 and 13 that the additions were located on the eastern side, on the new land recently purchased. In the absence of original building plans or plans for the 1936 works it is interesting to speculate if the front of the building was remodelled or simply extended at this time to accommodate the extension.



**Figure 13:**  
**Aerial photograph over**  
**subject site, 1943. Site**  
**outlined in red with**  
**arrow. Note: there is now**  
**a sawtoothed factory**  
**building on the eastern**  
**part of the subject site.**  
SIX Maps.

Figure 14 provides an undated photograph of the front elevation of the building, likely to date from the c.1940s. The parapet is noticeably absent. When this was added to the

<sup>23</sup> "Tenders Called", *Construction and Real Estate Journal*, 22 July, 1936, p.8.

building is unknown. Note the shutters to the first floor windows, since removed. The ambulances are identified as two Packards, a 1936 Buick, a 1935 Buick and a Humber.

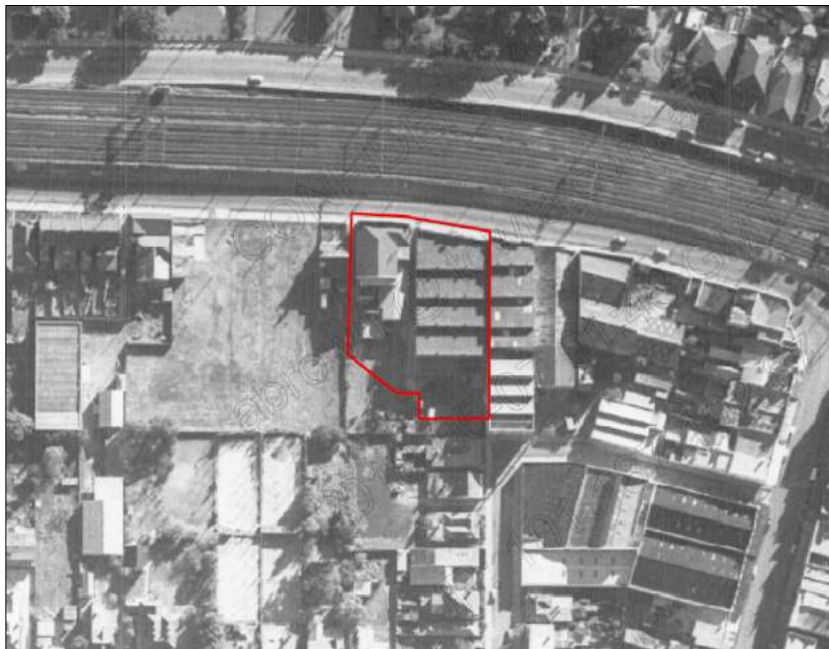


**Figure 14:**  
Undated  
photograph  
of the front of  
Summer Hill  
Ambulance  
Station.

Provided by  
the Inner West  
Library.  
Original  
source: John  
Gerdtz, *Buick  
in the  
Ambulance  
Service of  
NSW.*

The earliest plans that Council hold for this building relate to additions carried out in 1955 and 1958. These additions were located to the rear and have since been demolished.<sup>24</sup>

Figure 15 is an aerial photograph dated 1961 after the alterations of 1956. Note: the demolition of buildings to the west of the site and the beginnings of the creation of the adjoining Reserve.



**Figure 15:**  
Aerial  
photograph  
over subject  
site, 1961.  
Cited in JBS &  
G, *Health  
Infrastructure  
Phase 1 and  
Phase 2  
Environmental  
Site  
Assessment,  
Summer Hill  
Ambulance  
Station,*  
October, 2017.

Further alterations were carried out to the building in 1962 and 1972. These works were to the rear of the building and have since been demolished.<sup>25</sup>

<sup>24</sup> File Nos. 55/1676 and 58/2522. Ashfield Council Archives.

<sup>25</sup> File Nos. 62/4175 and 72/8607. Ashfield Council Archives.

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## 2.6 The Remainder of the Site Before 1980

The eastern half of the subject site was acquired by the Health Commission of NSW (later Health Administration Corporation) on 7 March, 1980.<sup>26</sup>

The history of this part of the site is only briefly described for the purposes of this statement.

This part of the site forms part of Lot 38 and Lot 39 Section 7 of the Underwood Estate subdivision. By 1881, the land was owned by Sydney victualler, Augustus Dewhurst.<sup>27</sup> Thereafter followed a succession of owners until, by 1895, the land was owned by Ellen Adeline McQuillan. McQuillan retained ownership of the site until 1926, when it was sold to John Henry Fraser.<sup>28</sup> The aerial photograph of 1930 above (Figure 8), shows that this part of the subject site was vacant land at this time. As noted above, a small section of this land was transferred to the Ambulance Board in 1935 and the Ambulance Station extended.

The remainder of the subject site was sold by Fraser to Grace Brothers Limited on 27 January, 1937 and leased it soon after to Feather and White Pty Ltd.<sup>29</sup> Advertisements indicate that this company were involved in the manufacture of hosiery. An advertisement for seamers and examiners for ladies hosiery at their 'New Daylight Mill' 'right at Summer Hill Station' was placed in *The Sydney Morning Herald* on 21 July, 1937 suggesting that the factory may have been newly constructed at this time.<sup>30</sup> This factory is clearly visible in the 1943 aerial photograph (Figure 13) and is partially visible in the c.1940s/1950s photograph taken at street level (Figure 14).

Feather and White purchased the site from Grace Brothers in July 1945, retaining ownership until selling it to the Health Commission of NSW in 1980.

## 2.7 Later Site History

A comparison of aerial photographs taken in 1978 and 1986 respectively (Figures 16 and 17) showing the extensive works undertaken by the Ambulance Service in the years after they acquired full ownership of what is now the subject site. The former Feather and White factory building on the eastern part of the site was either demolished and replaced or extensively altered and added to. The early skillions to the rear of the Interwar period Ambulance Station were demolished and replaced with the existing structure.

The surrounding area also change following the demolition of dwellings to the south of the subject site and their replacement with a carpark (1978) and later a building (1986). A concreted sports area and tennis courts have been constructed to the west of the original Ambulance Station.

The western boundary of the site was altered in 1987 when the Health Administration Corporation purchased a small strip of land along this boundary from the Council of the Municipality of Ashfield.<sup>31</sup>

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<sup>26</sup> From title information provided in JBS & G, *Health Infrastructure Phase 1 and Phase 2 Environmental Site Assessment, Summer Hill Ambulance Station*, October, 2017, Appendix F.

<sup>27</sup> See Certificate of Title Volume 666 Folio 207. NSW LPI.

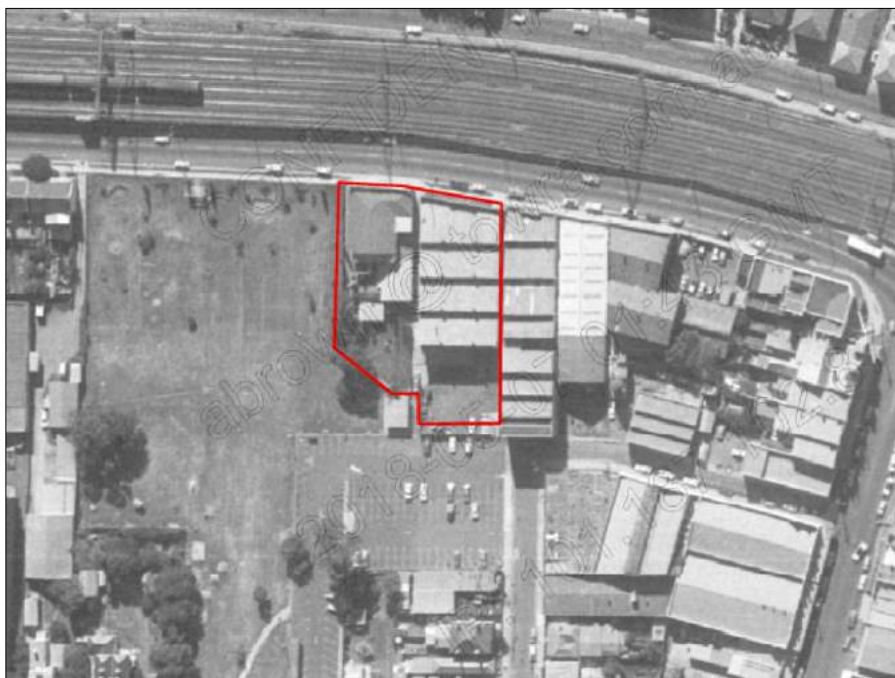
<sup>28</sup> Transfers attached to Certificate of Title Volume 666 Folio 207. NSW LPI.

<sup>29</sup> Certificate of Title Volume 4751 Folio 25. NSW LPI.

<sup>30</sup> 'Knitting', *The Sydney Morning Herald*, 21 July, 1937, p.26.

<sup>31</sup> From title information provided in JBS & G, *op.cit.*, 2017, Appendix F.





**Figure 16: Aerial photograph over subject site, 1978.**

Cited in JBS & G, *Health Infrastructure Phase 1 and Phase 2 Environmental Site Assessment, Summer Hill Ambulance Station*, October, 2017.



**Figure 17: Aerial photograph over subject site, 1986.**

Cited in JBS & G, *Health Infrastructure Phase 1 and Phase 2 Environmental Site Assessment, Summer Hill Ambulance Station*, October, 2017.

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### 3.0 SITE ASSESSMENT

#### 3.1 The Site

For the following, refer to Figure 18, a recent aerial photograph over the site, and to the survey that accompanies this application.



**Figure 18:**  
**Recent Aerial**  
**photograph over the**  
**site.**

NSW Lands  
Department, 2016.

The site is located on the southern side of Carlton Crescent. It is irregular in shape, with a northern boundary of Carlton Crescent of 44.6m and a maximum site depth, along the eastern boundary, of 38.1m. The site area is 2,896.5sqm. The natural fall of the site is to the south.

The buildings described below occupy the majority of the site area. There is a narrow accessway along the western boundary, which leads into an irregularly shaped rear yard. There is a wire fence along the western boundary and a high brick wall to the south western corner splay. A high wire fence on a painted cement block base runs along the rear boundary. The high walls of the adjoining building to the east form the boundary of the rear yard on this side. The accessway and the rear yard are concreted. There is no significant planting on the site.

Figure 19 illustrates the rear yard.



**Figure 19:**  
Looking east across the rear yard.

### 3.2 Building 1 (No. 75 Carlton Crescent)

Building 1, the Ambulance Station opened in 1924, occupies the north western corner of the site. As set out above, it has undergone a number of alterations since this time.

#### 3.2.1 Exterior

The original section of this building is two storeys in height and is constructed of rendered masonry and face brick. It has a pitched roof clad in terracotta tiles.

The principal elevation is the **northern elevation**, addressing Carlton Crescent. This elevation is two storeys in height and is constructed of rendered and painted masonry. The elevation rises into a simply detailed stepped parapet that conceals the roof from the street. The parapet features a small plaque in the centre with a Maltese Cross, being the symbol of the ambulance service.

There are four irregular spaced openings at ground floor level. The two eastern-most openings are large rectangular openings with modern automatic garage doors. Continuing west, there is a recessed opening with simple metal grill. Within the opening there is a pair of timber framed and glazed doors with toplights. The western-most opening has a pair of timber framed doors with toplight and modern security grill.

At first floor level, there are five timber framed double hung windows with six panes to each sash. Whilst the timber shutters in visible in Figure 14 have been removed, the shutter clips remain. A wide raised rendered band runs across the elevation, beneath the windows. A shallow hood, tiled in unglazed terracotta Cordova tiles, runs across the elevation above the windows and below the parapet.

Note in the photographs below the decorative moulded border that runs above the ground floor openings and down the outer sides of the eastern and western most openings. There is a second, and different, decorative band beneath the hood. The words 'Western Suburbs District Ambulance' lie below the windows and above the decorative banding at ground floor level.

Figures 20 to 24 illustrate the front elevation.





**Figure 20: Front elevation.**



**Figure 21: Detail of the main pedestrian access into the building. The floor is finished in no-original quarry tiles.**



**Figure 22: The western most opening.**



**Figure 23:**  
Detail of the first floor, showing the windows, shutter clips, lettering, decorative banding below the hood, cordova tiles to the hood and the Maltese Cross in the parapet.



**Figure 24:**  
Detail of the decorative banding at ground floor level.

The **western elevation** of the original building is constructed of common brick laid in stretcher bonding. The eaves are wide and timber lined with exposed rafters. There are five openings of varying sizes at ground floor level. These openings are fitted with timber framed double hung windows with one pane to each sash. They have moulded brick sills and rendered and painted lintels and are fitted with modern security sills.

There are four windows matching the detailing of the ground windows (without the security grills) at first floor level. There is also a large opening at the northern end, fitted with three timber framed casement windows with two panes to each sash. It is noted that the two most northern openings are set higher in the wall than the southern three



openings. Figures 25 to 28 illustrate the western elevation. Note the later services attached to this elevation.



**Figure 25:**  
Western elevation.



**Figure 26:**  
Detail of the timber lined  
eaves with exposed rafters.



**Figure 27:**

**Detail of one of the windows with rendered lintel and moulded brick sill. Note also the terracotta wall vents.**

**Figure 28: Metal vent at sub floor level.**

The **southern elevation** of the original building is largely concealed by the 1980s addition to the rear. Where visible above the addition, the elevation is constructed of painted brickwork. There are six timber framed double hung sash windows with one pane to each sash at first floor level. There is a timber vent set into the wide gable. The eaves are wide and timber lined with supporting timber brackets. Refer to Figure 29.



**Figure 29:**  
**First floor of the rear elevation of the original building, southern elevation.**

The short return of the **eastern elevation** at the northern end, where the elevation stands clear of the adjoining building, is rendered and painted. This elevation is blind at ground



floor level. At first floor level there is a timber framed double hung sash window with six panes to each sash. The front parapet returns the corner to conceal the hipped roof form above. Refer to Figures 30 and 31.



**Figures 30 and 31: Northern end of the eastern elevation.**

Figures 32 and 33 illustrate the exterior walls of the deck located to the rear of the building at first floor level. The detailing of the eastern facing wall, including the window, matches the western elevation. The original doors and windows in the southern wall have, however, all been replaced with aluminium framed doors and windows. The deck has a timber floor and a timber balustrade.



**Figure 32:  
Eastern facing wall  
addressing the rear deck.**



**Figure 33:**  
Southern facing wall  
addressing the rear  
deck.

A single storey extension was constructed to the rear in the 1980s. The extension has face brick walls and a low pitched roof clad in corrugated metal. The western elevation is blind. There is a covered dock at the southern end. Refer to Figures 34 to 37.



**Figure 34:**  
Western elevation of  
the 1980s extension.



**Figure 35:**  
South western corner of  
the 1980s addition.



**Figure 36:**  
Southern elevation of the  
1980s addition.

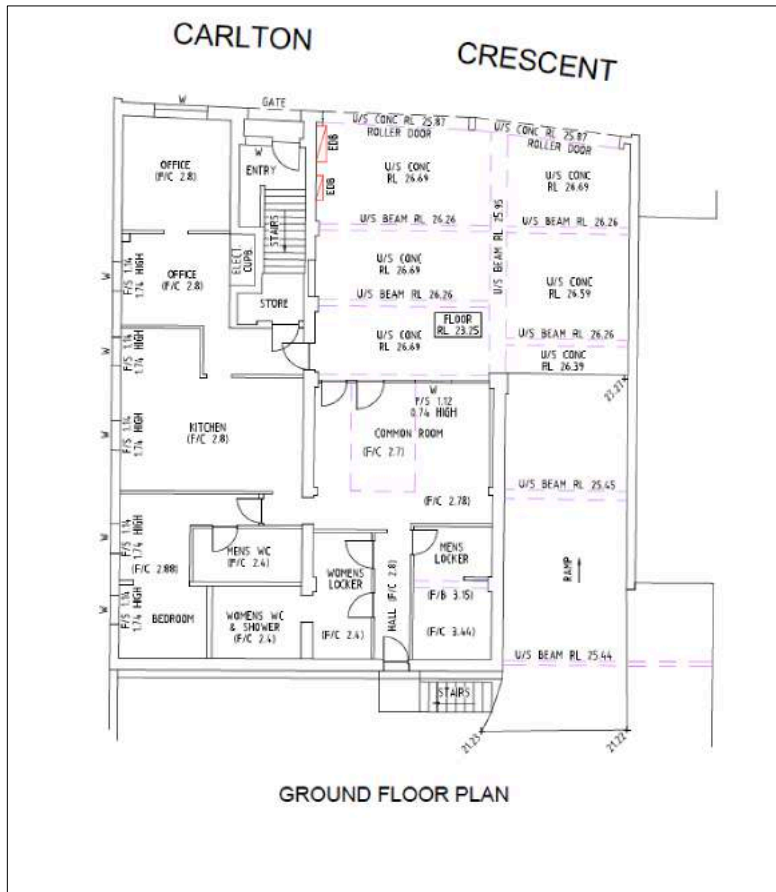


**Figure 37:**  
Roof over the 1980s  
addition.

### 3.2.2 Interior

#### Ground Floor

The ground floor plan of the pre-1980s section of the building is best understood with reference to Figure 38.



**Figure 38:**  
**Ground Floor Plan.**  
LTS Lockley.

The garage in the north eastern corner of the building and the ramp leading to the rear along the eastern side have a concrete floor and rendered and painted masonry walls. Doors leading into the office-accommodation rooms at this level are single panel doors. Figures 39 to 42 illustrate these spaces.





**Figures 39 and 40: Looking south into the building from the entrances off Carlton Crescent.**



**Figure 41:  
Looking north up the ramp,  
showing the outer wall of  
the men's locker room and  
common room.**

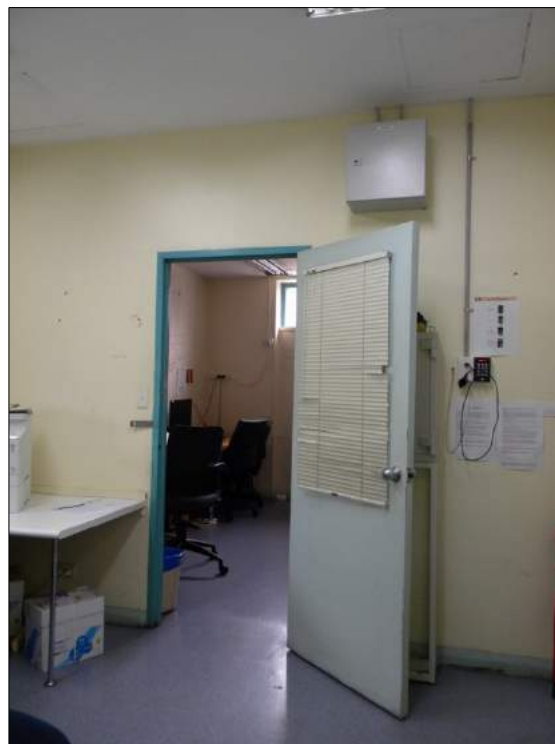


**Figure 42:**  
Rear wall of the offices  
from within the later  
garage addition.

The finishes in the office-accommodation rooms at this level are simple. Walls are masonry and plastered and painted or framed plasterboard walls. Floors are concealed by a variety of finishes. Inspection beneath the building has shown that while the garage area has a concrete floor, these rooms have a timber floor structure. There are simply profiled timber skirting boards and architraves (varying or modified profiles). The door panels are single panel doors. Most ceilings have been lowered and are square set. The kitchen and bathroom facilities are modern. Figures 43 to 48 illustrate typical interiors at this level.



**Figure 43:** Office in the north western corner of the building.

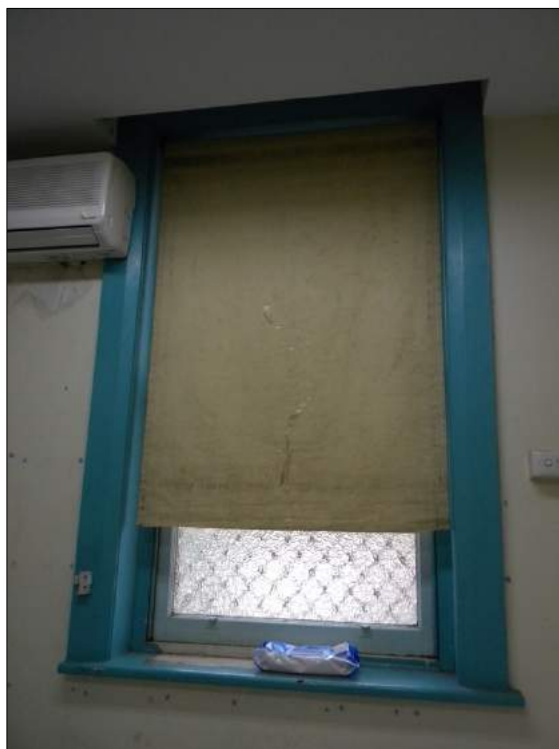


**Figure 44:** Southern wall in the above office. This appears to be a later wall and door.





**Figure 45: Modern kitchen facilities. Note how the ceiling height has been lowered.**



**Figure 46: Detail of one of the original windows. Note how the ceiling height has been lowered.**



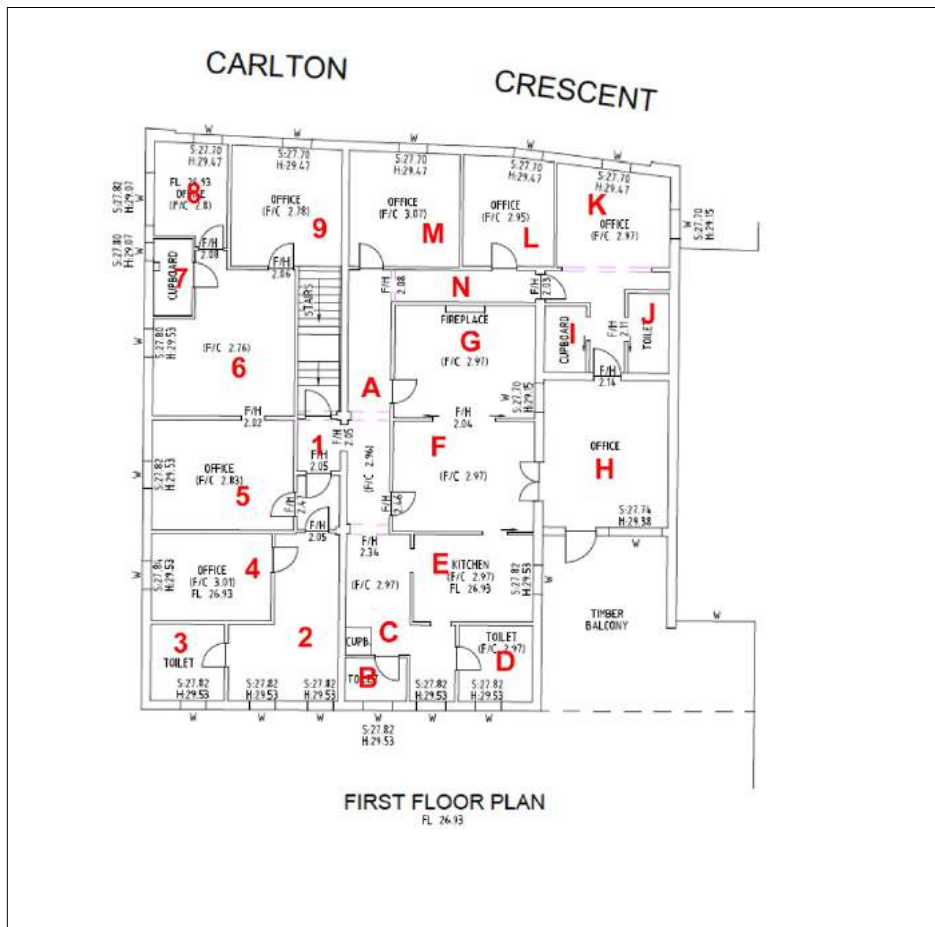
**Figure 47: Locker Room with exposed services.**

**Figure 48: Common room.**

The 1980s addition is a large garage area with a concrete exposed brick walls and unlined roof.

## First Floor

The first floor plan of the building is best understood with reference to Figure 49.



**Figure 49: First Floor Plan.**

LTS Lockley; annotation in red by WP Heritage.

Figure 50 illustrates the staircase leading from Carlton Crescent to the first floor. The stairs are covered in carpet; there is a modern hand rail to one side. The ceiling appears to be a plasterboard ceiling with quad cornice.

The first floor level currently divided into two sections. The smaller western section, most recently used as an office (identified by the numbers in Figure 49 above), and the larger eastern section, most recently used as a residence (identified by the letters in Figure 49 above).

The floors on the western side (Rooms 1-9) are carpet over timber boards, with the exception of the bathroom, which is tiled. Walls are predominately masonry, plastered and painted; there are a number of timber framed and plasterboard clad walls. These rooms (with the exception of the bathroom) have simply profiled timber skirting boards, picture rails and architraves. Original and later skirting boards, picture rails and architraves have different profiles. Original door openings have toplights, now all blocked in or painted. Original doors are high waisted panel doors. Later doors, or in some instances, replacement doors, are single panel doors. Most ceilings are lowered drop-in panel ceilings. There is a battened fibro (or equivalent) ceiling in Room 3, being the bathroom, and in Room 4. The bathroom fit out is mixed in date. Figures 50 to 59 illustrate this side of the first floor.



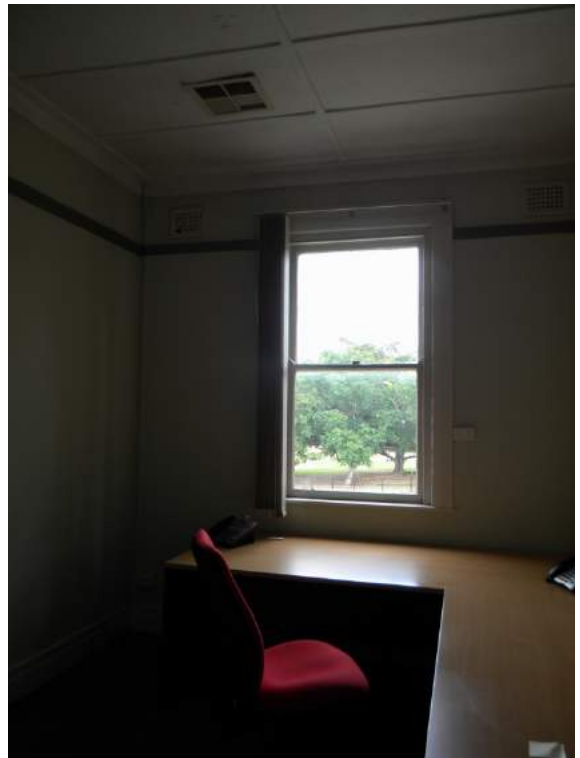
**Figure 50: Stairs leading up to the first floor.**



**Figure 51: Door leading from Room 2 into Room 1. An earlier opening has been partially infilled and the door added.**



**Figure 52: Original skirting boards, picture rail and windows in Room 2.**



**Figure 53: Earlier, likely original, battened ceiling in Room 4.**



**Figure 54:**  
Original door with blocked in toplight and later  
door panel (Room 5).

**Figure 55:**  
Room 6, looking north.



**Figure 56:** Original door panel into Room 8.

**Figure 57:** Casement windows in Room 8.



**Figure 58:**  
Example of original and later skirting boards  
in Room 9.

The rooms on the eastern side of the first floor (Rooms A-M in Figure 49) have similar finishes. The ceilings are, however, plasterboard ceilings with a cove cornice. The bathrooms in Rooms B and D have modern fit outs. The kitchen fit out in Room E is mixed in date. There is a modern folding door with stained glass insert in the southern wall of this room. Figures 59 to 64 illustrate typical finishes and spaces on this side of the first floor.



**Figure 59: Double doors in Room F.**



**Figure 60: High waisted panel door with original hardware in Room G. The fanlight is blocked in.**





**Figure 61: Fireplace in Room G.**

**Figure 62:**

**Formerly outside wall in Room H, looking back into Room F.**



**Figure 63: Room M.**

**Figure 64: Hallway (N).**

### **3.3 Building 2 (No. 74 Carlton Crescent)**

This building has not been inspected in detail for the purpose of this statement. It presents to the street as a two storey c.1980s light industrial building. Refer to Figures 65 and 66.



**Figure 65:**  
The building on  
the eastern half  
of the site from  
Carlton Crescent.



**Figure 66:**  
The building on  
the eastern part  
of the site.

### **3.4 The Surrounding Area**

#### **3.4.1 The General Area**

For the following, refer to Figure 67, an aerial photograph over the site and the surrounding area. The site is located within a mixed residential, commercial, recreational and light industrial area.



**Figure 67: Aerial photograph over the site and the surrounding area.**  
SIX Maps; Annotation by WP Heritage.

### 3.4.2 Carlton Crescent

Carlton Crescent follows the line of the railway line on its southern side from Old Canterbury Road in the east to Liverpool Road in the west. The road, in the vicinity of the site, is narrow. There are concrete kerbs and gutters to either side. There is a narrow footpath on the northern side, above which rises the high brick retaining wall and the railway track. There is wider footpath, with narrow nature strip, on the southern side. There is one street tree outside of the subject site.

This section of the street is mixed in character. Adjoining the site to the east, is No. 72 Carlton Crescent, a light industrial building. The building on this site is built to its front and side boundaries. It presents a two storey face brick late Interwar period Functionalist Style elevation to Carlton Street. The factory to the rear extends to the rear site boundary. Continuing east are two other Interwar period factory buildings and the three storey Victorian period Summer Hill Hotel on the corner with Lackey Street. All these buildings are built to their Carlton Crescent boundary.

To the west of the site lies the Summer Hill skate park and hard surfaced tennis courts. Continuing west lies the tree lined open space of the Darrell Jackson Gardens.

Figures 68 to 73 illustrate Carlton Crescent in the vicinity of the site.





**Figure 68:**  
The railway along  
the northern side  
of Carlton Crescent.



**Figure 69:**  
No. 72 Carlton  
Crescent to the  
east, an immediate  
pre/post World  
War II industrial  
building.



**Figure 70:**  
Western side of No.  
72 Carlton Crescent,  
adjoining the site  
to the east, as it  
addresses the site.  
This building  
covers its site area.



**Figure 71:**  
Skate park  
adjoining the site  
to the west.



**Figure 72:**  
Tennis courts to  
the west of the site.



**Figure 73:**  
Daryl Jackson  
Gardens.

### 3.4.3 South of the Site

South of the site lies a narrow pedestrian pathway that leads through to Hardie Avenue. A substantial single storey building housing a supermarket lies on the opposite side of the laneway. Refer to Figure 74.



**Figure 74:**  
**Pathway to the**  
**south of the site.**

## 4.0 ASSESSMENT OF SIGNIFICANCE

### 4.1 Summary of Existing Citations and Listings for the Site

No. 74 Carlton Crescent:

- Is located within the Summer Hill Conservation Area as defined by Schedule 5 Part 2 of the *Ashfield LEP 2013*.

It is noted that the site is:

- Is not listed as a heritage item by Schedule 5 Part 1 of the *Ashfield LEP 2013*.
- Is not listed as a heritage item on the State Heritage Register under the auspices of the *NSW Heritage Act 1977*.

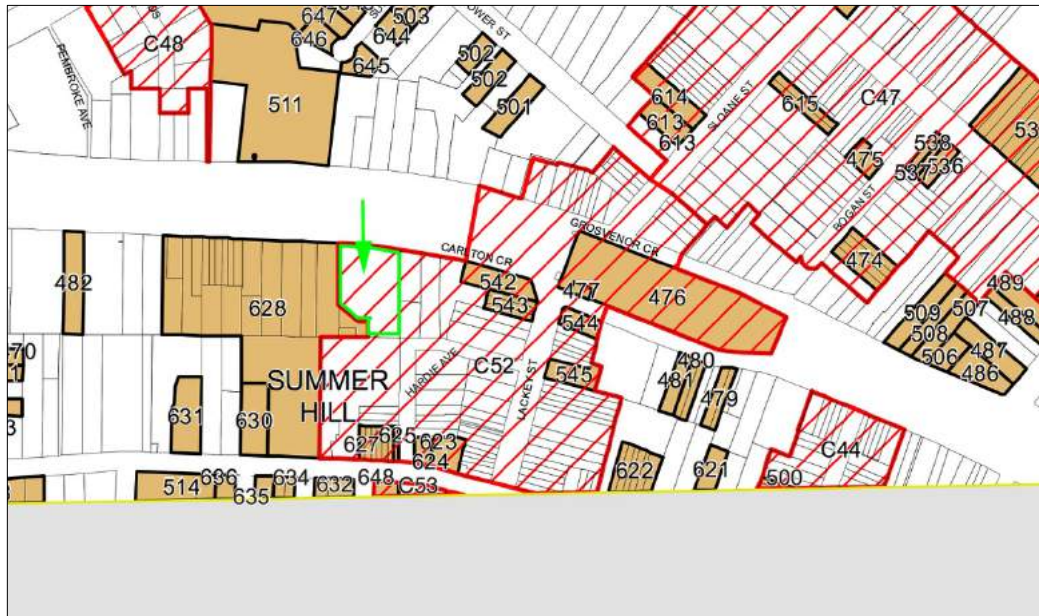
### 4.2 Heritage Items in the Vicinity of the Site

For the following, 'in the vicinity' has been determined with reference to physical proximity, existing and potential view corridors and the nature of the proposed works.

There are no heritage items listed on the State Heritage Register, under the auspices of the *NSW Heritage Act 1977*, in the vicinity of the site.

Figure 75 shows the location of heritage items, listed by Schedule 5 Part 1 of the *Ashfield LEP 2013*, and Conservation Areas, listed by Schedule 5 Part 2 of the *LEP 2013*, within the vicinity of the site. Heritage items are coloured brown or green and numbered. Conservation Areas are hatched in red and numbered. The subject site is outlined in green and marked by the arrow.





**Figure 75: Detail of the Ashfield LEP 2013 Heritage Plan.**  
Ashfield LEP 2013.

The following heritage items listed by Schedule 5 Part 1 of the *Ashfield LEP 2013* are located within the immediate vicinity of the site:

- Darrel Jackson Gardens (public reserve), No. 127-131 Smith Street, Summer Hill (1628).

This item adjoins the site to the west and south west and is illustrated by Figures 71 to 73 above. The State Heritage Inventory listing sheet describes this item as follows:

'A large park of irregular shape, extending from Smith Street on the south to Carlton Crescent on the north and containing a number of diverse components. The land was originally traversed by a creek, and so the park is in a slight hollow. At the Smith Street end it is narrower, flanked by the Community Centre on the west and the supermarket complex on the east. The entrance has the single sign identifying the gardens, while the stone plinth on which are mounted the two commemorative plaques is located here. A walkway leads down the gentle slope past the Community Centre and its small carpark towards the main grassed area. On the right there is a formal area with playground equipment. Beyond this the park widens to its full east-west extent, from the Ambulance Station at 75 Carlton Crescent on the east to the residential property at 93 Carlton Crescent on the west. A considerable area to the east is concreted for a pair of tennis courts, an extended basketball court and a skateboarding facility, while the western area is grassed and treed. A path leads southwards from a lichgate motif at Carlton Crescent, past the skateboard area and tennis courts, on past a barbecue and picnic area and some scattered playground equipment on the west, and then up to Smith Street. The ground cover around the barbecue, beneath tree canopies, consists of wood chips which, because the area is the lowest in the park, has suffered from washaway which appears to be exacerbated by stormwater drain blockages. The rear boundaries of the Smith Street properties on the west give a stepped edge to the park. One of these properties is the Council's Before and After School Care Centre, which opens on to the park and, like the Community Centre, has a balcony overlooking it. The School Care Centre is accessed from the rear of 135 Smith Street.



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The character of the park leaves much to be desired. While the grassed areas are pleasant, the concreted areas, though desirable and functional, are rather bleak, a result partially of their dull colours, which include the black-painted railings and the high mesh tennis court screens: a marked contrast with the Smith Street end of the park. The graffiti on some of the walls facing the park are also unpleasant. The overall impression bespeaks ad hoc rather than planned development.’<sup>32</sup>

The State Heritage Inventory provides the following statement of significance for this item:

‘This is Summer Hills most central garden and recreational area, possessing considerable but yet largely unrealised aesthetic potentiality. It has an interesting history and is a memorial to one of Ashfield’s most eminent citizens, the longest-serving alderman in municipal history and mayor for three terms.’<sup>33</sup>

The *LEP 2013* provides the following curtilage for this item: Lot 32, D.P. 378; Lot 2, D.P. 1009898.

The following heritage items are further removed from the site, but still located within its vicinity. The proposed works have the potential to form part of their wider setting and/or be visible in distant view corridors from these items:

- House and Former Stable Block No. 133 Smith Street, Summer Hill (I630).
- Hotel, No. 1 Lackey Street, Summer Hill (I542).
- Shop, dwelling, office, Nos. 1, 3 and 5 Lackey Street, Summer Hill (I543).
- Shop and dwelling, No. 111 Smith Street, Summer Hill (I625).
- Shops and dwellings, No. 113-123 Smith Street, Summer Hill (I627).
- House, No. 99 Carlton Crescent, Summer Hill (I482).
- Nurse Accommodation, No. 52 Grosvenor Crescent, Summer Hill (I511).

## **4.3 Integrity and Comparative Analysis**

### **4.3.1 Integrity**

The site boundaries have changed over time. There are no significant landscape elements.

The Ambulance Station has a long history of alteration and addition over time. Without historic plans or photographs of the building from when it was first erected, it is difficult to ascertain what the original front elevation looked like. Evidence suggests that was extended to the east in 1937 and a parapet added sometime after the 1940s. Despite these changes, the removal of the original garage doors and doors/windows from the ground floor openings and the removal of the shutters to the first floor, the building presents a coherent Interwar Mediterranean Style front elevation to Carlton Crescent.

The western elevation appears to be substantially intact, excluding the later security screens, which are intrusive.

The rear of the building has undergone alteration and addition over time so that there is no understanding of the original rear elevation at ground floor level. The rear

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<sup>32</sup> *Daryl Jackson Memorial Gardens (public reserve)*, No. 12-131 Smith Street, Marrickville. State Heritage Inventory Database No.: 1020258.

<sup>33</sup> *Ibid.*

structures erected before the 1970s were later demolished and replaced by the extant structure. The brickwork of the original building at first floor level may once have been face brickwork. Aluminium framed doors and windows have been added to the openings onto the rear deck at first floor level.

There have similarly been internal alterations over time. These are generally distinguishable in the form of lightweight (as oppose to masonry walls), differently profiled joinery and obviously modern ceiling and floor finishes.

#### 4.3.2 Comparative Analysis

The original Ambulance Station is typical of the type of building erected as ambulances stations in the Sydney area in the 1920s and 1930s. These buildings were typically two storeys with garage, office and living quarters. They were usually constructed by funds raised locally and were often designed by local architects and built by local builders. Examples of stations of similar sizes and dates are provided by Marrickville, Paddington and Auburn (refer to Figures 76 to 78). Many of the ambulance stations of this period have been closed and adapted for new uses.



**Figure 76:**  
**Auburn Ambulance**  
**Station erected in**  
**1931. Locally**  
**listed.**

WP Heritage, 2018.



**Figure 77:**  
**Paddington**  
**Ambulance Station.**  
**Locally heritage**  
**listed.**

WP Heritage, c.2015.



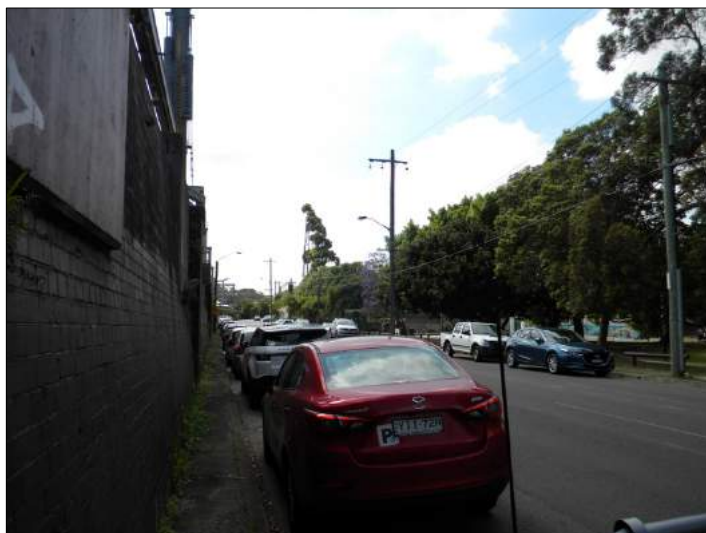
**Figure 78:**  
Former  
Marrickville  
Ambulance Station,  
c.1930. Not listed.  
Google Maps.

#### 4.4 View Corridors

The significant views towards the site are those that include the significant elevations of the original Ambulance Station. Views towards the building on the eastern part of the site are not significant.

The principal view corridors towards the original building are obtained when standing directly outside of it on Carlton Crescent. See the photograph on the front cover.

On approach along Carlton Crescent from the west, the original building is concealed until close by the site and at a wide angle by the slight bend in the street and by No. 74 Carlton Crescent. Closer to the site, there are raking views across the front elevation. Within the skate park, there are unobstructed views towards the western elevation (refer back to Figure 25). Refer to Figures 79 to 81.



**Figure 79:**  
View towards the site on  
approach from the west.



**Figure 80:**  
Closer to the site on  
approach from the west.



**Figure 81:**  
Diagonally opposite the site  
on approach from the west.

On approach along Carlton Crescent from the east, the original building is concealed until close by the site by the trees in the adjoining park. Closer to the site, the western elevation, which is a secondary elevation, becomes visible. At wider angles, there are raking views across the front elevation. Refer to Figures 82 to 84.



**Figure 82:**  
View towards the site on  
approach from the east.  
Google Maps.





**Figure 83**  
View towards the site on approach from the west.



**Figure 84:**  
Diagonally opposite the site on approach from the west.

The western elevation, roof and/or rear elevation of the original building are visible from many points within the adjoining park. These views are secondary to the views obtained from Carlton Crescent. Refer to Figures 85 to 88.



**Figure 85:**  
View towards the rear of the site from the adjoining Reserve.



**Figure 86:**  
View towards the rear of the  
site from the adjoining  
Reserve.



**Figure 87:**  
View towards the rear of the  
site from the adjoining  
Reserve.



**Figure 88:**  
View towards the rear of the  
site from the adjoining  
Reserve. This photograph  
was taken up towards Smith  
Street.

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## 4.5 Significance

### 4.5.1 The Conservation Area

*The Inner West Comprehensive Development Control Plan for Ashbury, Ashfield, Croydon, Croydon Park, Haberfield, Hurlstone Park and Summer Hill 2016 (DCP 2016)*, provides the following statement of significance for the Summer Hill Central Heritage Conservation Area:

‘The Summer Hill Central Heritage Conservation Area is of local heritage significance.

The Summer Hill Central area is of historical significance as an area of retail streetscapes developed in the period from 1878 through to the 1940s, in response to lobbying for and the actual opening of the Summer Hill Railway Station in 1879.

The area is of aesthetic significance for its varied mix of predominantly retail buildings dating from 1878 to the 1940s, illustrating architectural styles including Victorian Italianate, Victorian Filigree, Federation Free Classical and Inter-war Functionalist, unified by building alignments to the street frontage and awnings over the street, and predominantly 2 storey building heights.’<sup>34</sup>

This statement is adopted for the purposes of this assessment.

### 4.5.2 Contribution of the Site to the Conservation Area

The key period of significance within the Conservation Area is identified as being 1878 to 1940s.

The buildings within Conservation Areas are ranked according to their contribution. No. 74 Carlton Crescent, being the building on the eastern part of the site, is identified as ‘Post-War War II International’ and ranked as ‘2.’ No. 75 Carlton Crescent, being the original ambulance station, is identified as ‘Interwar Mediterranean’ and is ranked ‘1.’ The *DCP 2016* provides the following definitions:

#### Contributory Building 1:

‘Buildings that clearly reflect a Key period of Significance for the HCA and are key elements of the character of the HCA. Contributory 1 buildings generally have a good level of intactness in their external form and materials with only visible minor changes.’

#### Contributory Building 2:

‘Buildings that have been altered but are still identifiable as dating from a Key period of Significance for the HCA. Contributory 2 buildings have a greater level of change that may include roof additions, altered verandahs, changed wall finishes, altered windows and doors and sometimes a combination of these, however, these buildings have retained their overall form in the streetscape and remain part of the key street character.’

It is agreed that No. 75 Carlton Crescent should be ranked as ‘1’. The front and side elevations of the original building form, although altered over time, present coherent elevations that are relatively identifiable as being of the Interwar period. The building has also served a significant social function for the local community.

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<sup>34</sup> *Inner West Comprehensive Development Control Plan for Ashbury, Ashfield, Croydon, Croydon Park, Haberfield, Hurlstone Park and Summer Hill (DCP 2016)*, p.362.

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It is not agreed that No. 74 Carlton Crescent should be ranked as '2'. A comparison of the existing front elevation with the elevation shown by the c.1930s/1940s photograph Figure 14 (as well as existing aerial photographs of the roof form compared with historic aerial photographs) shows that the interwar period industrial building that once stood on this site has been substantially altered or completely demolished and rebuilt. Stylistically, the existing building on the site dates from the c.1970s/80s, which is well outside the time frame identified as being of significance to this Conservation Area. This building is more correctly ranked as '3' or '4' (neutral or detracting). Unlike No. 75 Carlton Crescent, there is nothing in the form and fabric of this building that specifically identifies its association with the ambulance service. It is not a particularly good example of any architectural style.

## **5.0 SCOPE OF WORKS**

The following should be read in conjunction with the plans prepared by Bates Smart that accompany this application.

- It is proposed to demolish Building 2.
- It is proposed to retain the front elevation and western elevation of the original section of Building 1 and part of the roof. It is proposed to demolish the remainder of the building.
- It is proposed to carry out the following works to the retained front elevation of Building 1:
  - Remove the existing doors/windows and garage doors and replace with new windows.
  - Reinstate the timber shutters to the first floor.
  - Paint the elevation.
- It is proposed to carry out the following works to the retained western elevation:
  - Install a new door at ground floor level.
  - Install two new windows at first floor level to match the existing.
- It is proposed to carry out the following works to the retained eastern elevation:
  - Cut a new door opening at ground floor level and install a new door.
  - Install an 'Iglu' sign at first floor level.
  - Paint the elevation.
- It is proposed to construct a new building behind the retained elevations of Building 1 and across the site. This building will range in height from two to four storeys, with the upper most level set into a recessed roof form. The new building will be setback from the Carlton Crescent boundary where it lies to the east of the retained elevations and will be U-shaped, providing for a central courtyard. Exterior finishes include face brickwork, off form concrete and steel and aluminium framed doors and windows. The roof of the new building will be clad in metal sheeting or concrete, subject to construction detailing.

New landscaping includes planting along the western boundary, within the central courtyard and at the rear of the site.

## **6.0 METHOD OF ASSESSMENT**

The following is a merit-based assessment. It does not consider compliance or otherwise with Council's numerical controls unless non-compliance will result in an adverse heritage impact. Refer to the Statement of Environmental Effects (SEE) that accompanies this application.



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The effect of work is assessed with an understanding of the relevant provisions of the *Ashfield LEP 2013 and the Inner West* and the *DCP 2016* in particularly Chapter D Part 8 Summer Hill Town Centre and Chapter E1 Part 3 Conservation Areas

The three questions raised by the NSW Heritage Division publication *Statements of Heritage Impact* (2002 update) have been taken into consideration.

## **7.0 EFFECT OF WORK**

### **7.1 On the Conservation Area**

Each aspect of the proposed works is assessed as to impact below.

#### **7.1.1 Demolition of Building 2**

This building has been ranked '2' by the *DCP 2016*. Under 3.2 of Chapter E1 Part 3 of the *DCP 2016*, contributory buildings are to be retained. As set out above, however, this building is more accurately ranked as neutral or detracting within the Conservation Area. A comparison of historic photographs and existing photographs (streetscape and aerial) demonstrate how the interwar period building known to have stood on this site has been extensively altered or, alternatively, demolished and the existing building erected in its place. It does not present to the Conservation Area as a building erected during one of the significant phases of development within the area and is not an architecturally distinguished example of a building from a later period. Unlike Building 1, there are no architectural elements which specifically relate this building to the ambulance service. Its demolition will have no impact on the heritage significance of the Conservation Area.

#### **7.1.2 Works to Building 1**

##### **With regard to the removal of part of this building:**

The primary objective for building ranked as '1' in a Conservation Area is that they be retained, conserved and maintained for their streetscape value. The controls stipulate the retention of the front or main section of contributory buildings, which is defined by the *DCP 2016* as 'usually the building below the main roof form.' The proposal retains the front and side elevations beneath the original roof form and part of the original roof form. It is not proposed to retain any of the floor structure behind. Removal of floors allows for a concrete structure which provides optimal fire and acoustic compliance, which is important given the number of people that it is proposed to house within the building. It also provides for BCA compliant sill height with regard to the retained windows.

Removing the additions to the rear will have no impact on the Conservation Area for the following reasons:

- These additions are c.1970s and later and have no architectural merit. They are located to the rear of the site and, while visible from the public domain, make no contribution to it.
- The controls for contributory buildings provide for the removal of later rear additions to contributory items where they are of no significance.

Removing the interiors will have an acceptable impact for the following reasons:

- The interiors of this building are not visible from the public domain.
- The interiors are not exceptional examples of interiors of this period and have undergone alteration.
- This building does not rely on the retention of its interior layout and finishes for its original purpose to be interpreted. The words 'Western District Ambulance Station' are emblazoned across the front elevation and the symbol of the ambulance service is included in the parapet.

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It is proposed to remove part of the roof to locate services. This is the best location for roof top services in terms of minimising the massing and scale of the new works. Services in this location will be concealed within the significant view corridors towards this building, which are from Carlton Crescent. View corridors towards this part of the building from the rear are not significant. The impact is acceptable for the following reasons:

- The roof is not a significant architectural element of this building. It is concealed by a parapet from the front elevation.
- The western plane of the roof is retained, preserving this roof form as the building is viewed from the west.
- The section of roof to be removed has minimal visibility from the public domain. View corridors which include this part of the roof are not significant to either the building or within the Conservation Area.

**With regard to the proposed works to the retained elevations:**

The existing pattern of openings at ground floor level of the front elevation, which dates from at least the late 1940s, is retained. This pattern of openings is significant because it relates to the use of the building as an Ambulance Station. Removing the existing doors/windows/roller shutters will have no impact. These are modern elements. Removing security grills from these openings will have a positive impact. The proposed new steel framed windows/doors are of a simple profile that will be identifiable as new fabric.

The proposed reinstatement of the timber shutters at first floor level of the front elevation will have a positive impact. These are an original element. Reinstating them will enhance the elevation.

The proposed works to the western elevation will have a minimal impact because this is clearly a secondary elevation. The proposed new windows have been designed to match the existing windows- in proportion and finish- and have regard to the alignment of existing windows. Additional openings in this elevation will not alter the way in which the building is read and understood within the Conservation Area.

It is proposed to create a new opening in the eastern elevation at ground floor level to provide a new main entrance into the building. This is a logical point of entry into both the retained building and the new works. An entrance in this location allows the front elevation of the original station to be retained without alteration. The impact is acceptable because this elevation is a secondary elevation. The proposed new doors has been appropriately proportioned. The simple detailing of the frame will identify it as new fabric.

The proposed new sign will have an acceptable impact. The placement of signage on this elevation alleviates the need to place signage on the front elevation of the building. It also locates the signage outside of the most important view corridors to the site, which are obtained from directly outside of it.

Repainting the building in the proposed colour scheme will have a positive impact. The colour scheme has been informed by the late 1940s photograph. It is not appropriate to 'pick out' the words 'Western District Ambulance Station' to avoid people misidentifying the building as a functioning ambulance station. These letters do not appear to have been picked out in the 1940s photograph. It is also for this reason that all signage associated with the ambulance service (excluding that forming part of the masonry fabric of the building) will be removed.

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### 7.1.3 The New Works

The proposed works will have an acceptable impact on the Conservation Area for the following reasons:

- The proposed new works are contemporary in style, as befitting infill development in a Conservation Area. This is an approach that preserves the integrity of the fabric record and is recommended by the *DCP 2016* controls. As set out below, the building nevertheless takes into account the form, scale, orientation and materials and finishes of the buildings in the immediate area.
- The proposed massing and scale will sit comfortably within the context of the immediately neighbouring buildings as seen from Carlton Crescent. As assessed above, it is from Carlton Crescent that the most significant view corridors towards this site are obtained. The group of buildings from the Summer Hill Hotel west to the subject site differs in character from the other buildings within the Conservation Area. These buildings are not the typical two storey Victorian through to Interwar period retail building that, according to the Statement of Significance, characterises this area. They are two storey large scale industrial buildings built to their street fronts on sites that vary in width.

The proposed new building will present to Carlton Crescent as being of a similar massing and scale to the building that it replaces. The increase in massing and scale proposed to the rear of the original Ambulance Station will not be read from Carlton Crescent. The upper most level is set back from the street front, managing massing and scale. The front elevation is also set back from Carlton Crescent, as for the existing. This not only provides a safe point of entry but retains the angled view corridor towards the site on approach from the east. It is noted that the existing building (and thus the proposed new works) does not block significant view corridors into, within or out of the Conservation Area.

- The proposal will increase the massing and scale of the built forms on the site as viewed from the west and south. The impact is acceptable because no significant view corridors into the Conservation Area will be blocked. The setback of the new building from the western boundary matches the setback of the original Ambulance Station. The existing views from these directions across the top of the original Ambulance Station, which will be reduced, does not contribute to understanding the significance of the Conservation Area. The specific impacts of the proposed massing and scale on the adjoining heritage listed park are assessed separately below.
- The form of the new building has been carefully considered. The form is simple, complimenting the generally simple parapeted forms of the light industrial buildings in this part of Carlton Crescent. The upper most level is set in a simple roof form, which is set back from Carlton Crescent.
- The proportions of the new elevations have similarly been carefully considered. The pattern and height of openings in the Carlton Crescent elevation compliments the pattern and proportion of openings in the original Ambulance Station. There are, for example, larger openings at ground floor level, echoing the garage entrances of the original Ambulance Station and smaller, vertically proportioned openings at first floor level, matching the alignment of the first floor windows of the retained building. There is a regularity to the proposed elevation that compliments the existing streetscape.

On the western elevation, the street wall height of the elevation of the new building matches the eaves height of the original Ambulance Station. The vertically proportioned openings similarly complement the western elevation of the original building.

- The proposed materials and finishes are contemporary but will complement the immediate streetscape. The proposed off form concrete provides a modern interpretation of the rendered wall finishes common in the area. Face brickwork is also a common finish. The dark metal roof cladding will complement nearby slate roofs and metal roofs.

- The proposed landscaping will contribute to Conservation Area as it matures, particularly the planting proposed along the western side of the site. This planting will partially screen the new works from the park.

## **7.2 On Heritage Items in the Vicinity**

### **7.2.1 Darrel Jackson Gardens (public reserve), No. 127-131 Smith Street, Summer Hill**

Note: the following does not consider impacts on amenity.

The proposed works will have no impact upon the ability to understand the historic significance of the park.

The existing setting of the park to the east and the view corridor out of the park across the subject site does not contribute to the ability to understand its historic or aesthetic significance. The view is of the secondary (often blind) elevations and roof tops of buildings fronting Carlton Crescent. The proposed building will provide a well-designed elevation that provides a distinct edge to the park. This elevation will be softened and partially screened by vegetation, which will contribute to the setting of the Park as it matures over time.

### **7.2.2 Other Items**

**House and Former Stable Block No. 133 Smith Street, Summer Hill (I630);  
Shop and dwelling, No. 111 Smith Street, Summer Hill (I625); and  
Shops and dwellings, No. 113-123 Smith Street, Summer Hill (I627)**

These items are located to the south and south west of the subject site and are separated from it by the park and/or other buildings. The proposed new buildings is likely to be visible from the rear of these items. The distance is sufficient for the proposed works not to dominate their setting or significant view corridors out of each item. It will not be visible within the most significant view corridors towards these items, which are obtained from Smith Street.

**Hotel, No. 1 Lackey Street, Summer Hill (I542) and  
Shop, dwelling, office, Nos. 1, 3 and 5 Lackey Street, Summer Hill (I543)**

These items are located to the east of the site and are separated from it by intervening buildings. The distance and massing and scale of intervening buildings is sufficient for works of the proposed nature not to impact upon these buildings. The proposed new building will not be visible in significant view corridors to or from these buildings.

**House, No. 99 Carlton Crescent, Summer Hill (I482)**

This item is separated from the subject site by the park and intervening buildings. It is sufficiently removed from this item for there to be no impact on the character of its immediate setting and no impact on view corridors.

**Nurse Accommodation, No. 52 Grosvenor Crescent, Summer Hill (I511)**

This item is located on the opposite side of the railway line. The upper levels of the new building will be visible from Grosvenor Crescent. The distance and visual separation provided by the railway line is sufficient for there to be no impact on the immediate setting or view corridors associated with this item.



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## **8.0 CONCLUSIONS**

This Heritage Impact Statement has outlined the history of No. 74 Carlton Crescent and established its contribution to the Conservation Area. The western part of the site is occupied by an Interwar Mediterranean Style Ambulance Station, opened in 1924 by the Western District Ambulance Service. The eastern half of the site is occupied by a large building, also used by the ambulance service, that presents as a c.1970s light industrial building. The original Ambulance Station building is contributory to the Conservation Area. The c.1970s building is neutral.

It is proposed to retain part of the 1924 building, demolish the c.1970s building and construct a new building, of up to four storeys in height, to provide student accommodation.

The proposed works retain those elements of the original ambulance station that contribute to the Conservation Area. The front elevation is improved through the replacement of non-original doors/windows at ground floor level and the reinstatement of the first floor shutters. The proposed new building will have an acceptable impact on the Conservation Area because it has been designed with reference to the orientation, form, massing, scale and character of this part of the Conservation Area. It will read as a well -designed infill building.

The proposed works will provide a distinct edge to the adjoining heritage listed park. Views of the side and rear elevations and roof forms of the light industrial buildings fronting Carlton Crescent will be replaced with a well-mannered building. It will not block significant view corridors into or out of the park. The proposed planting along the western side will contribute to the park as it matures.